

**Seattle Multimodal Project at Colman Dock
Season Three Marine Mammal Monitoring Report**

**Washington State Department of Transportation
Ferries Division**

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ATTACHMENTS

Marine Mammal Monitoring Plan (July 2019)

Marine Mammal Monitoring Observations (2019/20) Spreadsheet



1.0 Project Setting and Land Use

The Seattle Ferry Terminal at Colman Dock, serving State Route 519, is located on the downtown Seattle waterfront, in King County, Washington. The terminal services vessels from the Bainbridge Island and Bremerton routes, and is the most heavily used terminal in the WSF system. The Seattle terminal is located in Section 6, Township 24 North, Range 4 East, and is adjacent to Elliott Bay, a tributary to Puget Sound (Figure 1-1). Land use in the area is highly urban, and includes business, industrial, the Port of Seattle container loading facility, residential, the Pioneer Square Historic District and local parks.

1.1 Description of the Activity

WSF is proposing to preserve the Seattle Ferry Terminal at Colman Dock (Figure 1-1). The project will reconfigure the dock while maintaining approximately the same vehicle holding capacity as current conditions.

The reconfiguration would increase total permanent overwater coverage (OWC) by about 5,400 square feet (SF) (about 1.7% more than existing overwater coverage at the site), due to the new walkway from the King County Passenger Only Ferry (POF) facility to Alaskan Way and new stairways and elevators from the POF to the upper level of the terminal. The additional 5,400 SF will be mitigated by removing a portion of Pier 48, a condemned timber structure.

The project will remove the northern timber trestle and replace a portion of it with a new concrete trestle (Figure 1-2). The area from Marion Street to the north edge of the property will not be rebuilt and will become, after demolition, a new area of open water. A section of fill contained behind a bulkhead underneath the northeast section of the dock will also be removed. WSF will construct a new steel and concrete trestle from Columbia Street northward to Marion Street.

Construction of the reconfigured dock will narrow (reduce) the OWC along the shoreline (at the landward edge) by 180 linear feet at the north end of the site, while 30 linear feet of new trestle would be constructed along the shoreline at the south end of the site. The net reduction of OWC in the nearshore zone is 150 linear feet.

The project includes demolition of the existing terminal building and construction of a new terminal building. The new terminal building will be located along the west edge of the dock, spanning all three slips to handle passenger traffic more efficiently, and will connect to the Marion Street Overpass by an elevated deck.

The project includes reconstruction of the vehicle transfer span and the passenger overhead loading (OHL) structures of Slip 3, including new hydraulic systems. The new OHL would be wider than the existing OHL, to accommodate the increased walk-on passenger volumes.



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Figure 1-1 Location of Seattle Ferry Terminal

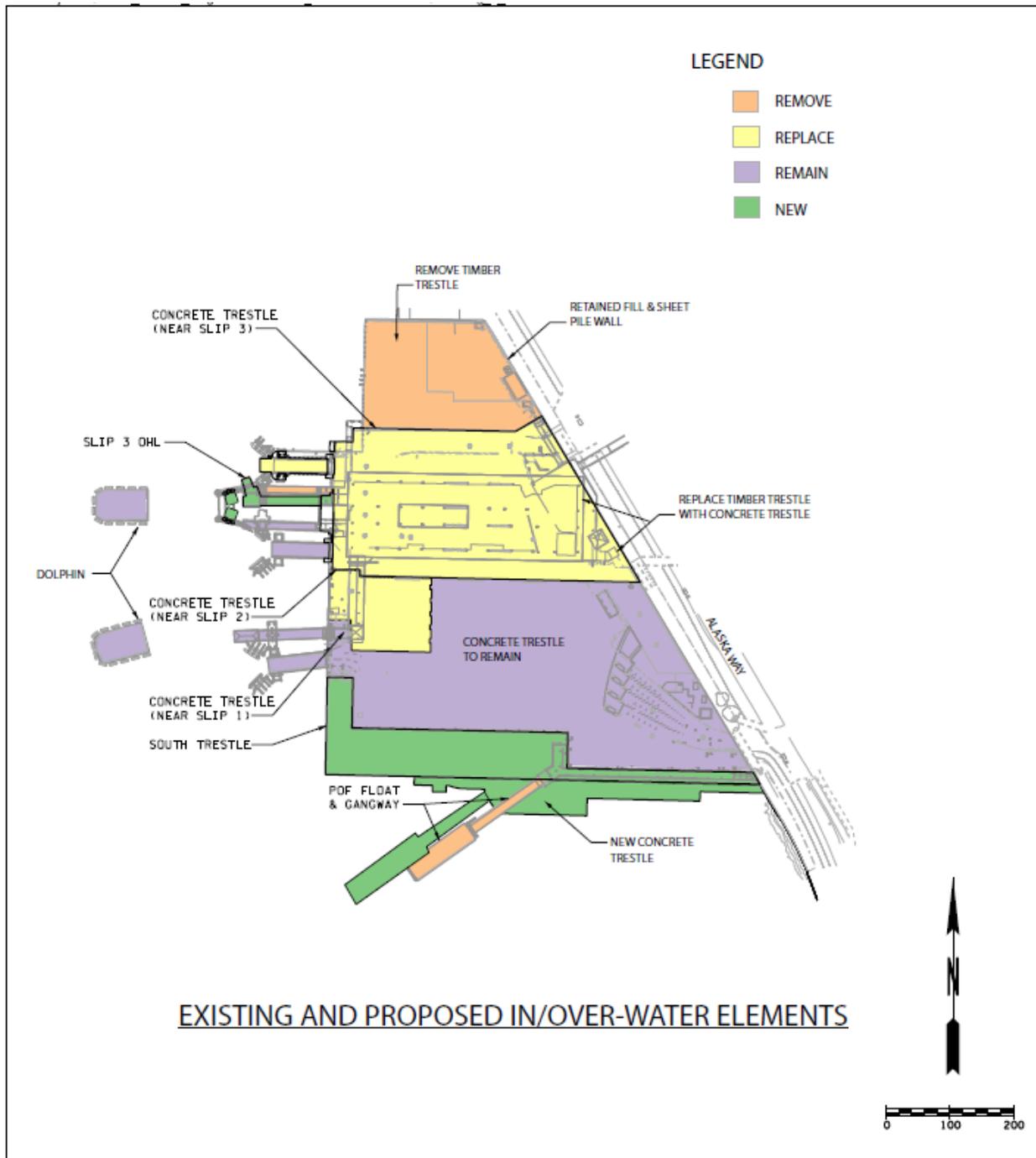


Figure 1-2 Existing/Proposed Construction Elements



The project will maintain the current POF functions on site, and address safety concerns related to pedestrian/vehicle conflicts. A new covered pier, sized to accommodate POF passenger waiting and connected by a new overhead pedestrian bridge to the terminal building and the Marion Street Overpass, would be constructed along the south side of Colman Dock

Sediment beneath the terminal has been contaminated by the creosote-treated piles and other chemicals discharged to the environment over the years. A cap was installed to cover contaminated sediment on the south half of the site prior to trestle expansion in 1990. WSF proposes to place a new sediment cap to the north and south of the current cap during construction of the project to contain existing contamination.

Stormwater management will be improved by the addition of Filterra treatment units in the southern portion of the terminal, which will remove oil and suspended solids.

The effects of the project were analyzed pursuant to the National Environmental Policy Act and the federal co-lead agencies, FTA and FHWA, issued a finding of no significant impact (FONSI) on November 5, 2015. During the NEPA process, the project underwent formal Endangered Species Act (ESA) consultation with National Oceanographic and Atmospheric Administration (NOAA) Fisheries and the U.S. Fish and Wildlife Service. NOAA Fisheries issued a Biological Opinion on March 20, 2014 and USFWS issued a Biological Opinion on February 18, 2014. An ESA re-initiation is now in process, primarily due to a change in pile type from concrete to steel, and is expected to be complete in the spring of 2017.

The construction will take approximately five years, beginning in mid-2017. The terminal will be kept in operation during the construction.

1.2 Construction Seasons

The project consists of four in-water work seasons:

- Season 1 (2017/18) construction activities focused on the South Trestle, Terminal Building Foundation, Pier 48 mitigation, temporary Passenger Offloading Facility and temporary work platforms.
- Season 2 (2018/19) included constructing the North Trestle, pedestrian walkway, permanent Passenger Offloading Facility, Slip 3 and temporary work platforms.
- Season 3 (2019/20) included construction of the new North Trestle, Slip 3 and temporary work platforms.
- Season 4 (2020/21) will include the demolition of the existing North timber trestle, completion of the new North Trestle and Slip 3, and temporary work platforms.



1.2.1 In-water Project Elements Completed in 2019/20 (Season 3)

Pile numbers that were planned and completed for Season 2 are provided below.

Table 1-1. Pile Numbers Planned/Completed Season 3

Permanent Structures	Permanent Installed	Permanent Removed
Slip 2 Bridge Seat	(8) 36-inch steel	(1,046) 14-inch timber*
Center Trestle	(93) 36-inch steel	(19) 14-inch steel H*
Slip 2 Wingwall	(2) 24-inch steel	(2) 18-inch concrete
Extension		(108) 12-inch steel
Slip 2/3 Inner Dolphin		(15) 18-inch steel
		(3) 36-inch steel
Temporary Structures	Temporary Installed	Temporary Removed
Templates	(148) 24-inch steel	(148) 24-inch steel

2.0 Monitoring and Take Results

Marine mammal monitoring was implemented for all pile driving and removal in the 2019/20 in-water work window (August 1 to February 15). An extension of the work window was approved by NMFS and USFW through February 29. Monitoring took place over 116 days, starting August 12, 2019 and ending February 29, 2020. Pile driving/removal was paused for 7 hours 31 minutes in Season 3 in order to avoid unpermitted take, prevent injury or to comply with visibility requirements for monitoring.

IHA reporting requirement 6a. (xi) states: Number of individuals of each species (differentiated by month as appropriate) detected within the monitoring zone, and estimates of number of marine mammals taken, by species (a correction factor may be applied to total take numbers, as appropriate).

No correction factor has been applied to the observed take noted in Table 2-2, as WSF has not been able to identify a method that can be used, and no guidance is available from NMFS regarding acceptable correction factors.

Permitted take, observations and take used are provided in Tables 2-1 and 2-2. In addition to the species observed listed in Table 2-2, nine unidentified dolphin or porpoise, one unidentified small whale, one unidentified large whale and 20 unidentified pinnipeds were observed.



Two unidentified dolphin or porpoise and seven unidentified pinnipeds were present in active Level B zones. For this report, it is assumed that these individuals were the most common of these animals, or harbor porpoise (two takes) and harbor seal (seven takes). All other unidentified individuals, were observed when no pile driving or removal was taking place, or they were outside of active Level B harassment zones.

The marine mammal monitoring plan, and sightings data, including behavioral observations, are provided as separate attachments.

Table 2-1. Permitted Take

Species	Level A	Level B	Total Take
Harbor Seal	114	1,492	1,606
Elephant Seal	0	15	15
California Sea Lion	0	2,628	2,628
Steller Sea Lion	0	175	175
Transient Killer Whale	0	30	30
Gray Whale	0	30	30
Humpback Whale	0	30	30
Minke Whale	0	10	10
Harbor Porpoise	103	335	438
Dall's Porpoise	64	208	272
Long Beaked Common Dolphin	0	49	49
Bottlenose Dolphin	0	49	49



Table 2-2. Observations and Take Used

Species	Individuals Observed	Total Take Used	Level A Used	Level B Used
Harbor Seal	678	190*	1	190*
Elephant Seal	0	0	N/A	0
California Sea Lion	763	1	N/A	225
Steller Sea Lion	28	9	N/A	9
Transient Killer Whale	29	0	N/A	0
Gray Whale	1	1	N/A	1
Humpback Whale	3	1	N/A	1
Minke Whale	1	0	N/A	0
Harbor Porpoise	157	49*	0	49*
Dall's Porpoise	3	0	0	0
Long Beaked Common Dolphin	0	0	0	0
Bottlenose Dolphin	0	0	0	0

*Includes two unidentified dolphin/porpoise and seven unidentified pinnipeds present in active Level B zones. It is assumed that these individuals were the most common of these animals, or harbor porpoise (two takes) and harbor seal (seven takes).



2.1 Data Collection

All data was collected in ArcGIS Survey 123. Data fields collected are listed below. All monitoring data is attached as Appendix B (electronic).

Table 2-3. Data Fields

Protected Species Observer Data Fields
PSO Monitor Name
Project
PSO Monitoring Station ID
Construction Activity
Weather Conditions
Specify other. (Weather)
Observation Date & Time
Species Observed
Specify other. (Species)
Duplicate Sighting
Number of Individuals Observed
Direction of Sighting from the PSO
Distance from the PSO
Compass Bearing towards Animal from PSO (optional data)
Distance from PSO to Animal (Meters) (optional data)
Compass Bearing to Noise Source from PSO (optional data)
Distance from PSO to Noise Source (Meters) (optional data)
Calculated Angle between the Bearings (optional data)
Distance of Animal from Noise Source (Meters) (optional data)
Observed Behavior
Direction of Travel
Comments about the Sighting
Zone Selection
Number of Individuals in Shutdown Zone
Number of Individuals in Harassment Zone
Harassment/Shutdown Comments



Appendix A
Marine Mammal Monitoring Plan
(provided as a separate file)



Appendix B
Marine Mammal Monitoring Data
(provided as a separate file)